

## Preparing to safely store pontoons in Grays Harbor

Pontoon moorage is a method of anchoring the pontoons for storage in Grays Harbor until they are needed for the bridge replacement. Because of their massive size, they must be stored and transported in water. The pontoon moorage area is located on the south side of the Grays Harbor navigation channel, and provides sufficient depth and protection from wind and waves.

When each batch of pontoons is complete, crews will flood the casting basin to float out the pontoons. Tugboats will tow the pontoons to the moorage location where they will be secured and stored. Pontoons will be moored until needed for the construction of the SR 520 floating bridge, beginning in 2012, and opening to traffic as soon as 2014. The pontoons will be towed from the moorage site to Lake Washington.



Pontoons will be towed to their moorage location by tugboat.

## Wetland and aquatic improvements at Grass Creek

Aberdeen resulted in a loss of about one acre of wetlands and shoreline habitat. To mitigate for this loss, we are providing improvements to wetland and aquatic resources at the 68-acre Grass Creek site in Grays Harbor County. The enhancements will strengthen the local natural environment by establishing wetlands, creating mudflats, removing invasive species, and reconnecting tidal channels. Work began in summer 2011, and will be fully completed in early 2013. WSDOT will monitor the site for 10 years.



Work at the site will reconnect tidal channels and restore salt marshes.

For more information:

Washington State  
Department of Transportation

**Pontoon Construction Project**  
1301 W. Heron Street  
Aberdeen, WA 98520

**Phone:** 360-500-4401

**Website:** [www.wsdot.wa.gov/projects/SR520/pontoons](http://www.wsdot.wa.gov/projects/SR520/pontoons)

**Email:** [pontoons@wsdot.wa.gov](mailto:pontoons@wsdot.wa.gov)

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### ADA:

Materials can be provided in alternative formats for people with disabilities by calling Shawn Murinko at 360-705-7097 or [murinks@wsdot.wa.gov](mailto:murinks@wsdot.wa.gov). Persons who are deaf or hard of hearing may contact Office of Equal Opportunity through the Washington Relay Service at 711.



## SR 520 Bridge Replacement and HOV Program



### Pontoon Construction Project

## Pontoon Construction Project in full swing

June 2012

### What is the SR 520 Pontoon Construction Project?

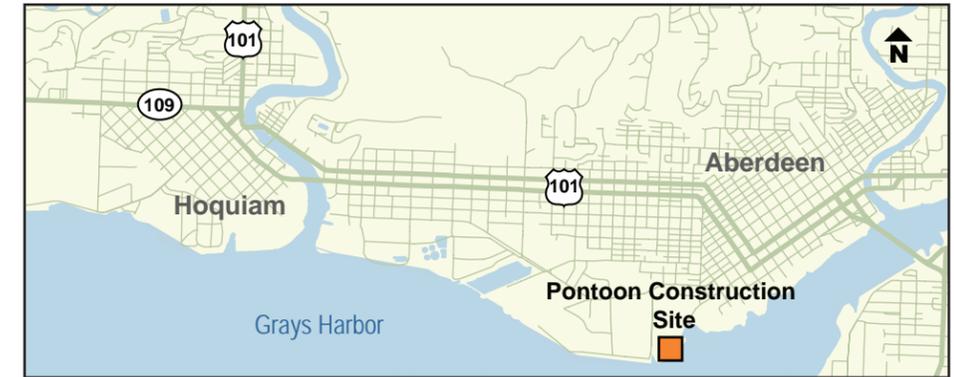
The aging SR 520 floating bridge on Lake Washington is vulnerable to windstorms and needs to be replaced.

WSDOT is moving forward with pontoon construction for a new SR 520 floating bridge.

The \$367.3 million Pontoon Construction Project broke ground in February 2011 at a 55-acre site in Aberdeen. The project contractor, Kiewit-General Joint Venture (K-G) has completed work on the pontoon construction facility and is building the first of 33 concrete pontoons for replacement of the SR 520 floating bridge.



Waves batter the SR 520 floating bridge during a winter windstorm.



Vicinity map

### Construction update

Casting basin complete! WSDOT and contractor Kiewit-General have built a casting basin facility at the 55-acre Pontoon Construction Project site in Aberdeen. The facility features a concrete batch plant, on-site water treatment and a 4-acre casting basin, where pontoon construction is under way. Work to complete the casting basin included:

- Excavating more than 280,000 cubic yards of soil from the site and leveling the base of the casting basin.
- Constructing the foundation by driving about 900 steel piles into the ground.
- Placing approximately 16,000 cubic yards of concrete to form the base slab, walls and crane rails.
- Installing casting basin gate in November 2011.
- Dredging a channel between casting basin and Grays Harbor in December 2011.

We are now under way with pontoon construction at the new facility, starting with the first cycle of six pontoons. The project's 33 total pontoons will be built in six cycles from now through 2014.



A rebar mat supports the base of the casting basin.



Crews place and level concrete to form the base slab of the casting basin.



## WSDOT brings jobs to Grays Harbor

We awarded a contract to Kiewit-General Joint Venture worth \$367 million. The contract includes designing and building the casting basin facility and 33 pontoons. We expect the Pontoon Construction Project to support hundreds of project-related jobs in Grays Harbor, including work in construction, supplies and supporting services.

Kiewit-General is responsible for all hiring for the project and working closely with unions to provide a skilled workforce for the project along with training opportunities. Several types of skilled trades and materials are needed for pontoon construction.

Skilled trades	Materials	Contractor contact information
<ul style="list-style-type: none"> <li>• Carpenters</li> <li>• Concrete workers</li> <li>• Crane operators</li> <li>• Electricians</li> <li>• Iron workers</li> <li>• Laborers</li> <li>• Machine operators</li> <li>• Painters</li> <li>• Truck drivers</li> <li>• Welders</li> </ul>	<ul style="list-style-type: none"> <li>• Asphalt</li> <li>• Concrete</li> <li>• Electrical</li> <li>• Fencing and gates</li> <li>• Job trailers</li> <li>• Lumber</li> <li>• Pumping equipment</li> <li>• Rebar</li> <li>• Scaffolding</li> <li>• Steel</li> </ul>	<p><b>Contractor:</b> Kiewit-General Joint Venture Phone: 360-291-8012 Website: www.kiewit.com</p> <p><b>Contract amount:</b> \$367 million</p> <p><b>Contract dates:</b> 2010-2014</p>

## Equal opportunities in construction

The Pontoon Construction Project contract includes several goals to encourage participation from disadvantaged, minority and women-owned business enterprises (DBE):

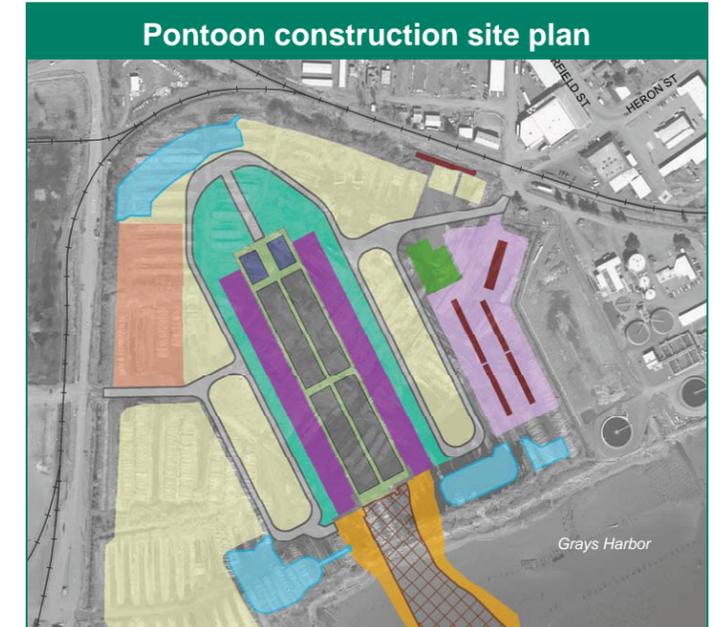
- 6 percent DBE goal for participation.
- 15 percent apprentice requirement.
- 50,000 training hours.
- State and federal prevailing wage requirements.

## What is the pontoon casting basin?

The casting basin is where the new pontoons are being built. The large basin uses about four acres of the 55-acre construction site in Aberdeen. Construction of the basin was completed in October 2011. After the pontoons are completed, crews will flood the basin and float the pontoons out into Grays Harbor.



Soundview Aerial Photography Inc., taken in 2011.



### LEGEND:

- Launch channel
- Rock side slope
- Concrete batch plant
- Supplemental stability pontoon
- Longitudinal pontoon
- Water treatment area
- Dry storage and laydown area
- Crane rail area
- Office
- Parking
- Casting basin
- Casting basin side slope



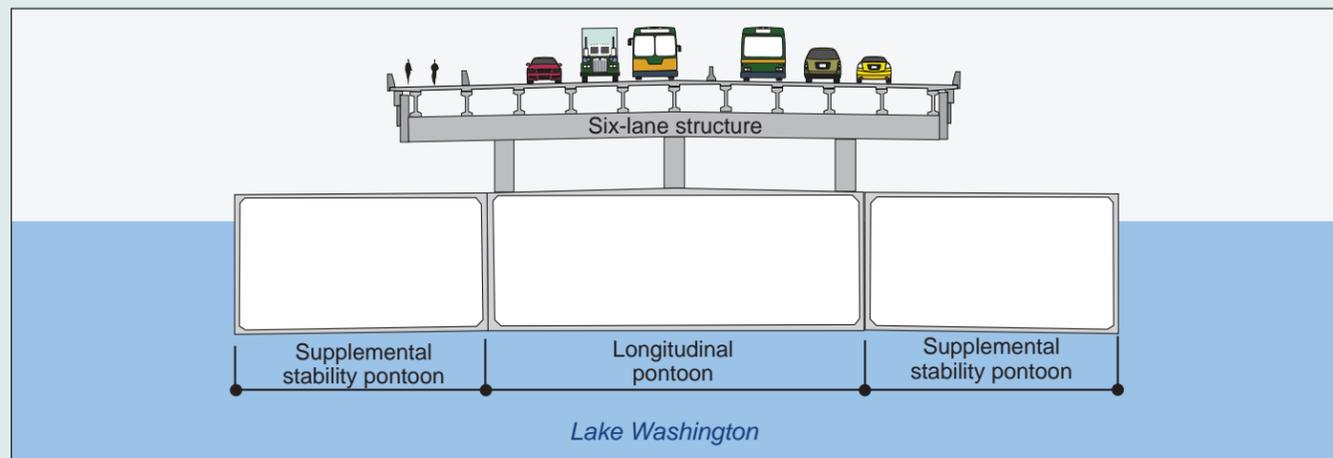
Crews excavated 280,000 cubic yards of soil to reach the depth of the casting basin floor.

## What are bridge pontoons?

Bridge pontoons are the foundation of a floating bridge. These large, hollow concrete structures are designed to support the weight of the road, plus the cars, trucks and buses that use the bridge every day.

SR 520 bridge pontoons will be up to 360 feet long – as long as a football field. They weigh a little over 11,000 tons – approximately equal to 23 Boeing 747 jets. In order to construct pontoons of this size, large quantities of material are necessary, including 112,000 cubic yards of concrete, 35,000 tons of rebar, and 2,745,000 square feet of formwork.

Pontoons are similar to a tanker ship or barge – even though they are very heavy, they can still float. This is because each pontoon displaces an amount of water that is more than the weight of the floating bridge.



Conceptual graphic of the proposed new SR 520 floating bridge showing the 4+2 lane configuration with two general-purpose lanes and one transit/HOV lane in each direction, and a bicycle/pedestrian path.

## Schedule

